



Illinois Tollway

ANNUAL REPORT





In 2020 we were confronted with a year unlike any other for the Illinois Tollway, our state and the world due to COVID-19. But despite the pandemic, we persevered to meet or exceed our goals as an agency keeping safety, customer service, diversity and infrastructure investments at the forefront of our daily activities, supported by our Board of Directors and Governor JB Pritzker's leadership.

Together we faced the pandemic head on, ensuring Illinois Tollway maintenance and operations continued unimpeded to serve essential workers and transport critical goods and supplies, while simultaneously fundamentally changing our approach to toll collection, including adopting policies fairer and more equitable to all our customers.

We implemented new procedures to keep our workforce safe. We prioritized our service to customers by halting cash collections at the plazas for safety and offered new options to pay tolls and reducing past due fines under an amnesty program. Our roadway team maintained 24/7 operations to meet the needs of our customers, and we awarded \$1.5 billion in construction contracts, the most for any year in Tollway history, including more opportunities for small and diverse businesses.

As 2020 continued, our traffic and revenue rebounded quite remarkably given the nationwide trends in tolling, transit and traffic. And, thanks to our agency's longstanding sound financial planning, we remain in a solid position to withstand these financial impacts allowing our capital program to proceed on track and our agency to remain committed to retaining our entire workforce.

None of this would be possible without our amazing Tollway staff, who serve our customers, maintain our system of roadways and work to coordinate infrastructure needs with state and local transportation partners.

All of these efforts have put us in the optimal position for 2021 to continue our commitment to project delivery, diversity, workforce development, and financial accountability, while doubling down on our efforts to deliver meaningful capital improvements to the region.

Executive Director

Jose R. Alvarez

OVERVIEW

The Illinois Tollway is dedicated to providing and promoting a safe and efficient system of highways while ensuring the highest possible level of service to our customers.

12 counties in Northern Illinois—Cook, Lake, DuPage, Will, Kane, McHenry, Boone, Winnebago, DeKalb, Ogle, Lee, Whiteside

294 centerline miles

5 toll roads—Tri-State Tollway (I-94/I-294/I-80), Reagan Memorial Tollway (I-88), Jane Addams Memorial Tollway (I-90), Veterans Memorial Tollway (I-90), Illinois Route 390 Tollway



TOLLING

The Tollway is a user-fee system—no state or federal tax dollars are used to support maintenance and operations. Operations are funded by toll revenues collected from our customers paying cash or electronically with their I-PASS or E-ZPass transponders. In 2020, the Illinois Tollway had:

- Nearly 1.3 million average daily drivers
- Average of 2.2 million daily toll transactions
- More than 806.6 million total toll transactions
 - 85% passenger vehicles and 15% commercial vehicles
 - 89.1% of all transactions paid via I-PASS/E-ZPass
- Nearly 5.4 million active I-PASS accounts
- Nearly 7.7 million active I-PASS transponders
- More than 556,000 Pay By Plate service users
- More than 37.5 million I-PASS transactions on E-ZPass roadways



TOLLING

Since the Illinois Tollway in June launched *TOLLING 2020*, a comprehensive tolling relief program, thousands of drivers have taken advantage of the changes to save nearly \$50 million by settling their unpaid toll violations at drastically reduced rates.

The Tollway reduced all outstanding \$20 and \$50 fines per toll to \$3 per toll, providing an opportunity for thousands of drivers to obtain relief. Through the end of December 2020, Tollway customers had cleared more than 180,000 outstanding toll violation notices making it the most successful relief package in the Tollway's history.

In the first 60 days following the launch, the Tollway cleared about 65,000 outstanding violation notices--about 7 times the number cleared in a 2009 amnesty program and nearly 6 times as many notices as were resolved during a 2005 amnesty.

TOLLING
A NEW WAY TO PAY.

A separate provision in this reform package allows drivers who accrued unpaid tolls at the start of the COVID-19 outbreak to avoid any fines and simply pay their missed tolls and a 1-cent invoicing fee. The Tollway invoiced about 4.7 million tolls accrued between March 9 and June 25, when the Tollway first closed cash lanes as a safety measure for customers and workers.

Also, moving forward as part of the reforms, customers are mailed an invoice with a \$3 fee per unpaid toll for passenger vehicles, a cost reduction of about 85 percent from the initial \$20 violation previously charged. And as always, all fees can be avoided by paying online within 14 days of the violation.

Responding to the strong public response to the existing violation relief program, the Illinois Tollway Board of Directors voted on Thursday, December 17, 2020, to continue the program for an additional six months until June 30, 2021.



CUSTOMER SERVICE

Whether it's I-PASS customer service centers, our website or keeping you safe with our roadway assistance, we are committed to delivering great benefits and services to customers.



1.9 Nearly
million
customer calls to **1.800.UC-IPASS**



40 Nearly
million
total website sessions in 2020

or

More than **170** thousand
website sessions a day

24 Nearly
million
total annual visits to getipass.com

15 More than
million
total annual visits to illinoistollway.com

572
Zero Patrol contacts

439
Heat Patrol contacts

34 More than
thousand
Calls to ***999**

131 More than
thousand
Incidents handled by
Tollway 911 Dispatch

155 More than
thousand
Calls received by
Tollway 911 Dispatch



CONTRIBUTING TO THE COMMUNITY

The Illinois Tollway is committed to supporting the communities we serve through participating in educational partnerships, supporting public involvement opportunities and engaging stakeholders.

- Hosted our 10th Annual Illinois Tollway Map Cover Art Contest—nearly 40 student artists from more than a dozen high schools throughout Northern Illinois submitted artwork in the contest, and nearly 4,700 votes cast on social media for winners.
- Through strategic partnerships with the Illinois High School and College Driver Education Association, the Illinois Tollway's winter driver safety curriculum was disseminated to more than 700 educators and 600 Illinois schools.
- Through a partnership with Distress Bandanna the Tollway's Give Them Distance message was shared with 1,525 students via 88 presentation (in person/virtual).
- Tollway planning and engineering staff participated in more than 315 outreach and interagency meetings to help coordinate projects and improvements systemwide.
- About 7,000 visitors after the Central Tri-State Tollway (I-294) Project Open House launched in August.



SAFETY FIRST

The Illinois Tollway is committed to better roadway safety. It is for this reason the Illinois Tollway has launched initiative after initiative to promote roadway safety, enhance the highway system and educate the public on the law and best practices of driving.

- In partnership with Illinois Department of Transportation and Illinois State Police, Tollway recognized week of April 20 as National Work Zone Awareness Week to ensure “Safe Work Zones for All.” Throughout Illinois, improving and maintaining infrastructure continues as an essential public service, with special care being taken to protect workers during the ongoing COVID-19 pandemic.
- National Safety Council
 - Teen Safety Award for newly established partnership with Illinois High School and College Driver Education Association and Distressed Bandanna Teen Driver Safety Initiative
 - Green Cross for Safety Award for safety innovation resulting in lower crash severity and reduced secondary crash rates.

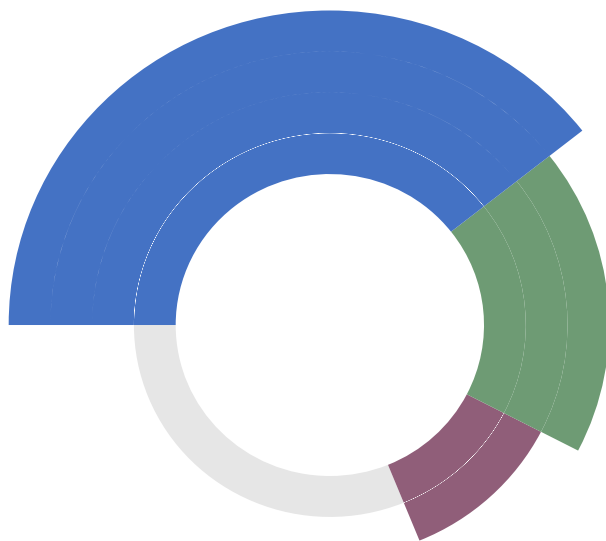
CONSTRUCTION

Move Illinois Program Update (2012-2020)

Move Illinois: The Tollway Driving the Future, is improving mobility, relieving congestion, reducing pollution, creating as many as 120,000 jobs and linking economies throughout the region.

The first nine years of *Move Illinois* are on schedule and within budget, delivering the new Illinois Route 390 Tollway and a rebuilt and widened Jane Addams Memorial Tollway (I-90) with its new SmartRoad corridor, as well as opening a new interchange connecting the Tri-State Tollway (I-294) to I-57. Progress continues on projects addressing the remaining needs of the existing Tollway system, delivering the new I-490 Tollway Project and reconstruction of the Central Tri-State Tollway (I-294).

- 52.5 percent of the program is complete (work in place)
- \$9.9 billion or 69.7 percent of the program budget was committed by the end of December 2020.
 - \$5.6 billion in construction work obligated/39.2 percent of program budget
 - \$2.6 billion in engineering consultant services has been committed/18.1 percent of the program budget
 - \$1.6 billion in other costs (utilities, right-of-way, etc.) has been committed/11.2 percent of the program budget



\$5.6 billion

in construction work obligated [39.2% of program budget]

\$2.6 billion

in engineering consultant services committed [18.1% of program budget]

\$1.6 billion

in other costs (utilities, right-of-way-etc) committed [11.2% of program budget]

remaining program budget

Railroad Agreements Pave the Way for New I-490

Negotiations for long-sought agreements, with Soo Line Railroad Company d/b/a Canadian Pacific and Union Pacific Railroad were approved by the Illinois Tollway Board of Directors. This represents the next major step forward for the I-490 Tollway Project. This project can now achieve its goals of improving access to O'Hare International Airport for businesses and communities and providing needed employment opportunities for workers."

The railroads will allow the mutual exchange of property rights necessary for construction of the new I-490 Tollway extending south from the Jane Addams Memorial Tollway (I-90) to the Tri-State Tollway (I-294) with interchanges creating new access into and around O'Hare International Airport and will enable improvements to rail operations in the Chicago Terminal area.

Construction of the new I-490 Tollway is designed to cross the Union Pacific Railroad at several locations, including where western access into and out of O'Hare International Airport is planned, as well as near the Tri-State Tollway (I-294) at the location of the future I-490/I-294 Interchange. The new I-490 Tollway is also will cross over the Canadian Pacific at several locations including where western access to the airport is planned and in the Canadian Pacific's Bensenville Yard. The project will require relocating Canadian Pacific trackage south of the yard.

The agreements provide the Tollway with property rights that will advance adjacent projects supporting construction of Cook County's planned improvements to Touhy Avenue, which include replacement of the at-grade crossing at the Union Pacific Railroad with a new bridge that will relieve congestion and improve



Work is moving forward to connect Illinois Route 390 and Illinois Route 490

access to the north side of O'Hare International Airport.

"The mutual benefits that will flow from these agreements are the result of cooperation and respect for one another's needs," said Mark Redd, Canadian Pacific's Executive Vice President of Operations. "We are excited that the forms of agreement will help the Tollway meet the needs of the traveling public of Northeast Illinois, and enable improvements to the rail network which will contribute to Chicago's economy and help position Canadian Pacific to meet future demand for freight rail service."

The new I-490 Tollway, scheduled to be complete by the end of 2025, coupled with the Illinois Route 390 Tollway which was completed in 2017, will

connect businesses and communities with one of the nation's busiest airports, transit facilities, major freight transportation hubs, distribution centers and multiple interstate highways including the Jane Addams Memorial Tollway (I-90) and the Central Tri-State Tollway (I-294).

Designed to accommodate access into the O'Hare International Airport, the two new all-electronic Tollways include 17 miles of new roads with 15 new or improved interchanges. The new I-490 Tollway will be built to carry north-south traffic around the western border of O'Hare and provide access to the airport. The Illinois Route 390 Tollway, which currently carries east-west traffic between Lake Street (Illinois Route 20) and Illinois Route 83, will include an interchange connection to the new I-490 Tollway.



AREA LEADERS' REACTIONS TO I-490

"The Elgin O'Hare Western Access Project will bring significant economic benefits to the region by creating jobs, reducing congestion, increasing connectivity, and improving transportation infrastructure in Illinois. After years of working with the delegation and the Tollway to advance this project, I'm pleased we are one step closer to finally getting this project across the finish line."

—**U.S. Senator Dick Durbin (D-IL)**

"The Elgin O'Hare Western Access Project is vital to the future of northeastern Illinois's transportation network, and today's vote is a critical milestone toward construction of this project. I'm encouraged by the progress between the Illinois Tollway, Canadian Pacific and Union Pacific Railroad, and I will continue to help this important project move toward completion."

—**U.S. Senator Tammy Duckworth (D-IL)**

"Today's announcement about the path forward for the Elgin O'Hare Project is, at long last, a significant win for our entire region. This agreement is the result of months, if not years, of hard work and good faith by all sides. The agreement will help ease travel around O'Hare, cut down on rail congestion that's bad for the freight companies and my constituents, and make access to the airport easier and smoother for thousands of Chicagoland residents."

—**U.S. Representative Mike Quigley (IL-05)**

"I'd like to thank the Tollway Board and staff for their diligent work on this matter. These negotiations have spanned three administrations, and the Governor and his team deserve credit for getting this agreement over the finish line."

—**Franklin Park Mayor Barrett Pedersen**

"This action brings DuPage County one step closer to realizing the benefits of improved transit to and from O'Hare Airport. Additionally, the completion of the I-490 Tollway will create thousands of jobs and economic opportunity on the west side of the airport, further benefiting our region. We welcome the agreements and we will continue to advocate with all parties involved toward improved western access to O'Hare International Airport."

—**DuPage County Board Chairman Dan Cronin**

"The Elgin O'Hare Western Access Project is among the most important economic and infrastructures projects in my district, and I'm very happy that the Illinois Tollway, Canadian Pacific, and Union Pacific Railroad are taking this crucial step forward. I am grateful to have helped facilitate the resolution of issues necessary to get to this point. Completing Western Access is a top priority for my constituents because of the thousands of jobs, increased tax revenue, and reduced traffic it will create for our region."

—**U.S. Representative Raja Krishnamoorthi (IL-08)**

"The Western Access Corridor Project has the potential to create over 65,000 jobs and reduce traffic congestion while creating a state-of-the-art regional transportation network that improves access to the nation's busiest airport. ACEC Illinois applauds the successful efforts of Illinois Tollway Chairman Will Evans and Executive Director José Alvarez for finalizing this critical agreement and advancing what is considered a "Project of National and Regional Significance" by federal transportation legislation. Because of the hard work and diligence of Tollway leadership, this essential project is now one-step closer to completion."

—**Kevin Artl, President and CEO of the American Council of Engineering Companies of Illinois**

Mile Long Bridge Halfway Complete

The Illinois Tollway opened the new northbound Mile Long Bridge to traffic ahead of the Thanksgiving holiday, marking the halfway point in the \$500 million construction project to deliver the new bridge and a major milestone for the Central Tri-State Tollway (I-294) Project. The northbound bridge contract is the largest in the Illinois Tollway's history.

The Mile Long Bridge Project includes replacement of two side-by-side structures—one to carry northbound traffic and the other to carry southbound traffic—to replace the original bridge built in 1958.

The \$184.6 million contract to build the new northbound bridge structure began in summer 2019. Work will continue over the winter under a second \$182.6 million contract to remove the old northbound structure and construct the new southbound Mile Long Bridge beginning in spring 2021. The entire Mile Long Bridge Project is scheduled to be complete by the end of 2023.

The Mile Long Bridge carries traffic over two major railroads, the Des Plaines River, the Chicago Sanitary & Ship Canal and the Illinois & Michigan Canal and local roads, and over several major distribution centers. Currently, up to 150,000 vehicles travel across the Mile Long Bridge daily.

The construction project includes building two, new side-by-side 4,800-foot-long bridge structures

designed to last 100 years and increasing capacity to five lanes in each direction. The inside shoulders in both directions will be built to serve as Flex Lanes for transit, for emergency vehicles and as an alternate lane when warranted.

The Illinois Tollway is coordinating work on the Mile Long Bridge Project with the Village of Hodgkins, Village of Countryside, Village of Willow Springs, Village of Justice, Cook County, Forest Preserve District of Cook County, Metropolitan Water Reclamation District, Burlington Northern Santa Fe Railway, Illinois Department of Transportation, Illinois Environmental Protection Agency, U.S. Army Corps of Engineers, U.S. Department of Fish and Wildlife, U.S. Coast Guard, Illinois Department of Natural Resources (IDNR), IDNR Office of Water Resources and the Illinois Nature Preserve Commission, as well as numerous businesses located near the Mile Long Bridge.

The project is part of the \$4 billion Central Tri-State Tollway (I-294) Project, scheduled for 2018 through 2025, to rebuild and improving the Tollway from Balmoral Avenue to 95th Street to provide congestion relief, improve mobility, reconstruct old infrastructure to meet current and future transportation demand and to address regional needs. This work is funded by the Illinois Tollway's 15-year, \$14 billion *Move Illinois* capital program.

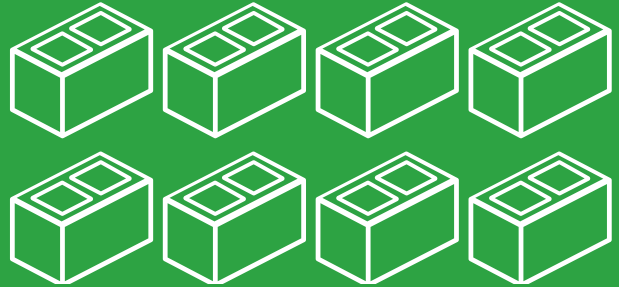


FACTS ABOUT THE NEW NORTHBOUND MILE LONG BRIDGE

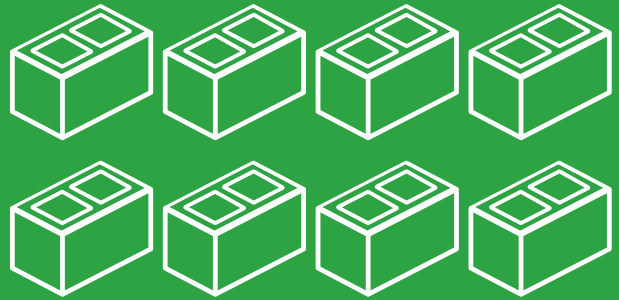


The bridge features **27** spans supported by **26** piers, compared to the existing structure with **53** piers, reducing the impact on waterways, roadway and rail operations below.

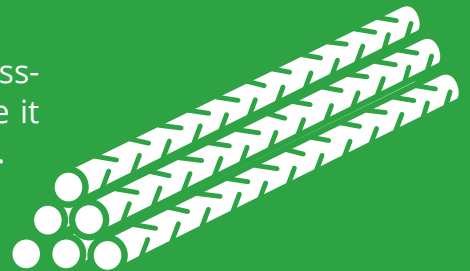
The bridge deck is supported by **273** precast concrete beams and **52** steel beams. The steel beams measuring up to 10 feet tall and ranging in length from **57** feet to **134** feet long. Concrete beams measure nearly 8 feet tall and are up to **187** feet long.



Nearly 16,000 cubic yards of concrete were used for the new bridge—an amount equal to about 1,760 fully-loaded concrete-mixing trucks.



The bridge deck includes 5.2 million pounds of stainless-steel rebar, which is being used on the project because it resists corrosion and helps extend the life of the bridge.





2020 Construction and Professional Services Contract Awards

Throughout 2020, all businesses, communities and the state were challenged by the economic impacts of the COVID-19 pandemic, by creating these opportunities the Illinois Tollway did its part to help keep businesses running and people working as we work to deliver roadway and infrastructure improvements to our customers

- In all, 75 construction and professional engineering services contracts totaling more than \$1.5 billion were approved by the Tollway Board in 2020.
- This included, 31 contract awards that provided opportunities for small, diverse and veteran-owned firms to take on roles as prime contractors and consultants.

By coming together with industry and responding with continuing critical investments in infrastructure in 2020 the Tollway awarded nearly 26 percent of all construction contracts and more than 62 percent of

all professional services contracts to DBE firms and advertised 45 small business contracts.

- African American-owned firms won construction contracts valued at \$37.6 million, an increase of nearly 150 percent from the \$15.2 million in construction contracts awarded to those businesses in 2019. Hispanic-owned construction businesses won contracts valued at \$158 million in 2020, a nearly 160 percent increase from the \$61 million in construction contracts awarded to Hispanic-owned firms in 2019.
- African American-owned design and engineering businesses in 2020 won contracts valued at \$38.4 million, nearly double the \$19.3 million in contracts they won the previous year, while Hispanic-owned design and engineering firms were awarded contracts valued at \$17.7 million in 2020, a 72 percent increase from the \$12.8 million in contracts won in 2019.



DIVERSITY PROGRAMS

The Illinois Tollway is committed to removing barriers and supporting ways to lift small, disadvantaged, minority, women and veteran-owned businesses and offering an opportunity to compete, with the ultimate goal of building capacity and opportunities for growth. However, even with great progress, the Tollway recognizes there is more work to do to ensure businesses and workers are as diverse as the communities we serve across Northern Illinois.

In 2020, the Tollway succeeded in creating more contract opportunities and investing in the support programs needed to grow the pool of available firms, and as a result, work delivered by small, disadvantaged and minority-owned firms increased significantly over the previous year. Examples of these achievements include:

Technical Assistance Program

167 bids submitted—34 firms winning work as primes or subcontractors on 25 contracts

Partnering for Growth (Construction)

4 new agreements with primes/protégés

Partnering for Growth (Professional Services)

5 new agreements with primes/protégés

Earned Credit Program

Total hires in 2020—30

Small Business Initiative

16 small business set-aside contracts awarded

ConstructionWorks

18 individuals hired

Building For Success Webinar Series

Nearly 2,000 attendees for 19 webinar events

SUSTAINABILITY

The Illinois Tollway is committed to fostering environmental responsibility and sustainability. Working together with communities and regulatory agencies, the Tollway strives to plan, design and construct roadway projects in accordance with local, state and federal regulations while using best practices to protect and enhance the natural and physical environment.

The Tollway is committed to building green and minimizing the environmental impact of construction by reducing, recycling and reusing materials. In 2020, Tollway projects included a variety of recycled material including:

- 211,477 tons of recycled asphalt pavement
- 147,848 tons of recycled concrete
- 3,594 tons of recycled fly ash
- 8,385 tons of slag
- 6,588 tons of asphalt shingles
- 80,958 recycled tires

In 2020, we planted approximately 4,050 trees and 7,270 shrubs towards our 58,000 goal.



International Bridge, Tunnel and Turnpike Association (IBTTA) announced the Illinois Tollway was awarded a 2020 Toll Excellence Award for Social Responsibility for its investments in 1,000 miles of local recreational trails and preservation of woodlands and wetlands.

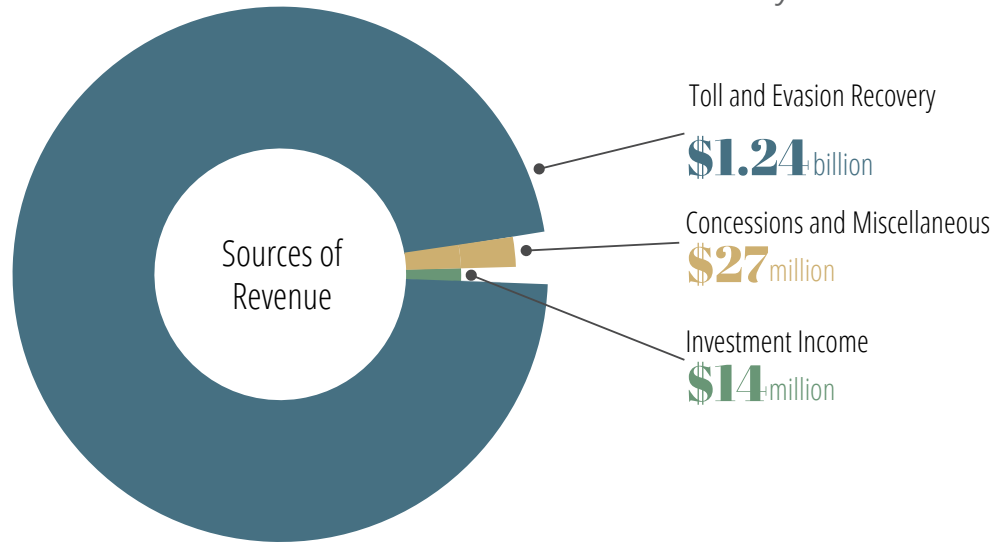


FINANCIALS

Preliminary and Unaudited

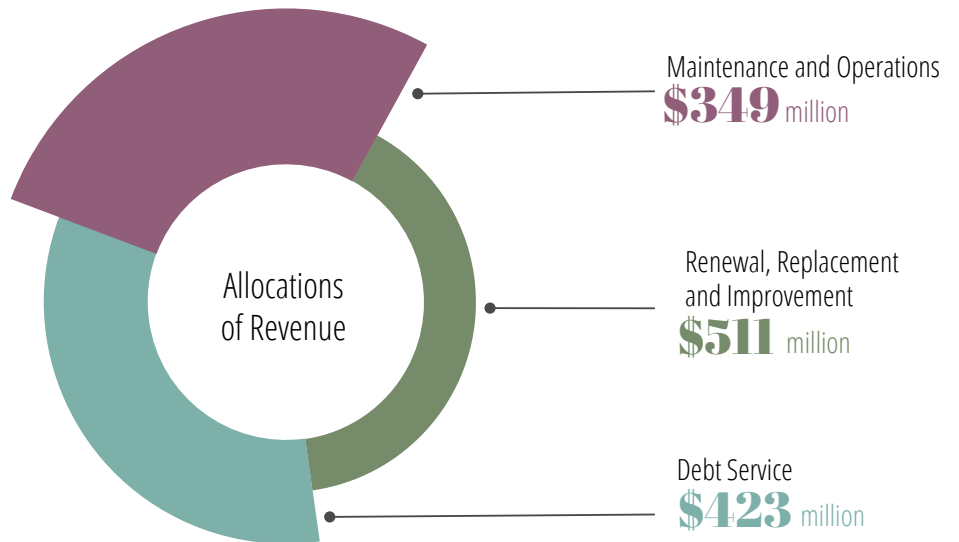
2020 SOURCES OF REVENUE

The Illinois Tollway is a user-funded system that receives no state or federal funding for maintenance and operations.



2020 ALLOCATIONS OF REVENUE

Allocations are made from revenue collected on a cash basis and may differ from revenue that is booked on an accrual basis.



\$1.17 BILLION IN CAPITAL PROGRAM EXPENDITURES

Capital expenditures are for roadway reconstruction, expansion and systemwide maintenance. Does not reflect reimbursements received pursuant to intergovernmental agreements. The capital program is also funded by proceeds from bond sales.

2020 financial information is preliminary and subject to change.



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